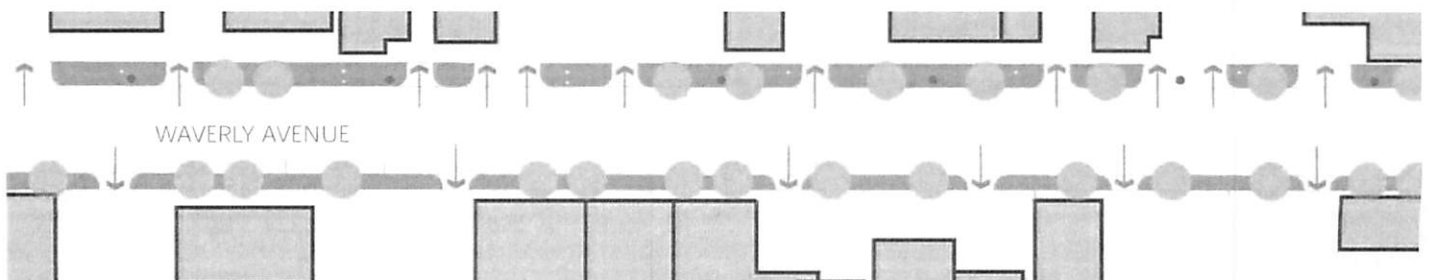


Waverly Avenue Design Study

Mamaroneck, NY

BFI Buckhurst Fish & Jacquemart Inc.

December 2004



DESIGN STUDY

For
***Waverly Avenue
Village of
Mamaroneck***

December 2004

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DECEMBER 2004

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Waverly Avenue Design Study Mamaroneck, NY

October 2004

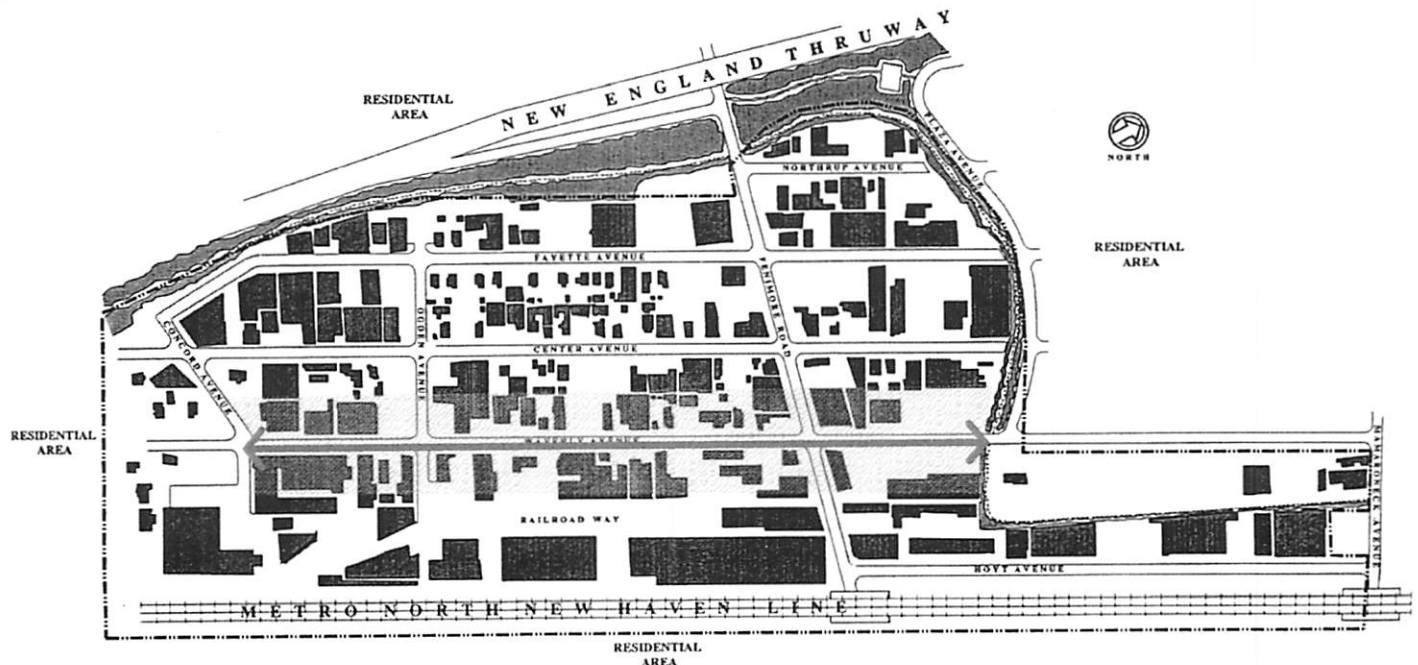
1. Introduction

The Village of Mamaroneck has a single M-1 industrial zoning district, which allows for multiple uses, including manufacturing, storage and warehousing, garages, repair shops and offices.

A previous study for the Westchester County Planning Office of the Mamaroneck Village Industrial Area (which includes this stretch of Waverly Avenue) estimated that 60% of the businesses involve manufacturing, multiple use or auto-related uses. Remaining uses include a scattering of non-conforming residential buildings and institutional uses.

The industrial area is roughly bounded by the Sheldrake River, Concord Avenue, the Metro-North Railway tracks, and Mamaroneck Avenue (dotted line on the figure below). Waverly Avenue is a major through street in the industrial area, and is the focus of the streetscape initiatives described in this report, although many of the ideas could be adapted to other streets in the industrial area.

This study takes into account the recommendations contained in the draft design report on the Fenimore Road Improvement Plan, prepared by Chas. H. Sells Inc. Particular note was taken of the engineering proposals related to the Fenimore Road/Waverly Avenue intersection.



2. Existing Conditions Analysis

Land Use

Waverly Avenue is a central street in the predominantly industrial area of Mamaroneck Village. The present zoning ordinance (1968) establishes the area around Waverly Avenue as an industrial district, and makes residential uses non-conforming. Over the years the number of residential properties along Waverly Avenue has decreased, but a few houses still remain today. In some cases, these properties are being used as accessory buildings for the adjacent commercial and light industrial businesses.

Most of the buildings on Waverly Avenue are occupied by manufacturing and auto-related uses. Major users include Carillo's Collision, and M. Argueso & Co, Inc., both of which occupy several properties on Waverly Avenue. Off-street private parking, truck storage, and loading also take up large sections of Waverly Avenue. Non-auto-related and non-manufacturing businesses include Fenimore Plumbing Showroom, Video Experts, Inc., Happiness Laundry & Dry Cleaners, and the Music School of Westchester. Multiple use buildings include industrial and auto-related uses in combination with other businesses, such as kitchen and bath showrooms, flooring producers and plumbing supplies. Multiple use buildings can be found all along Waverly Avenue, most with auto-related uses as one component business.

A large proportion of the street frontage along Waverly Avenue is taken over with parking and loading bays and outdoor storage areas. Off-street parking lots, loading docks and truck storage are in high demand throughout the industrial area, and frequently parking is not available, either on or off-street. A further discussion of parking needs is included in a following section of this report.

Street Edge Conditions

The edge conditions of Waverly Avenue change substantially between Fenimore Road and Concord Avenue. Along the southern edge of the road, sections of the street edge are poorly defined and are missing both sidewalks and curbs, encouraging cars to park indiscriminately within areas between the roadway and the building or property line. Some of these cars and trucks are double-parked, while others are intended for service at auto-related businesses, but are temporarily being stored on the street. Parking regulations are particularly difficult to enforce in areas where there is no striping for on street parking, and no hourly restrictions through metered parking.

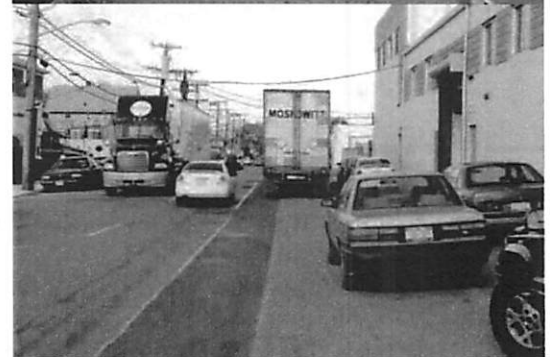
In other areas, there are curbs on the street but no sidewalks – or curbs with sidewalks that have been defined by yellow stripes painted on the paving. The lack of landscaping on the south side of Waverly Avenue also adds to the generally poor image presented by the Avenue.

Along the north side of Waverly Avenue, where the utilities lines are located, the street edge conditions are measurably better than on the south side. Much of this segment of Waverly Avenue has curbs, and for most of the block between Fenimore Road and Ogden Avenue, there are paved and defined sidewalks. Although there are few trees along this stretch of Waverly Avenue, the north side of the block between Rockland Avenue and Concord Avenue does have a few trees and some minimum landscaping. Examples include a few street trees, and a grass verge between the sidewalk and the curb, as shown on the right.

The lack of defined pedestrian space and the numerous driveways that feed onto Waverly Avenue make this area particularly difficult for pedestrians. The series of driveways along Waverly Avenue is hazardous to pedestrians and drivers, as parked cars along the street edge limit the turning visibility for cars.



Painted sidewalk on Fenimore Road



Cars and trucks parked along the shoulder of Waverly Avenue



Example of local streetscaping – grass verge between the sidewalk and the curb



Example of local streetscaping – grass verge with trees

Signage

Figure 1 – Existing Conditions – records the location of street signage that was observed in the Waverly Avenue Study area. Virtually all of this signage was related to parking and standing regulations, as opposed to directional signage, or informational signage about the area or local businesses. A list of signage examples is included below – however from observation, many of these signs went unheeded, with cars parked in unusual and illegal configurations along Waverly Avenue.

- No parking 12:01am-6:00am every 2nd Monday of each month
- Tow Away Zone
- No parking of commercial vehicles
- No stopping here to corner
- No parking anytime
- No parking this side of the street
- No parking 9pm-6pm
- Park Parallel
- 60 min parking 9am-7pm Monday-Saturday
- No standing anytime
- No parking 2am-6am except second Monday of each month
- No parking here to corner
- No standing here to corner
- No parking, loading zone
- No parking 2am-6am except 2nd Monday of each month

The signage that has been provided for business identification is intermittent and inconsistent. While a few businesses have prominent and legible signs, many properties are left unidentified. Of the ones that do provide signage, there is no consistency between signage types, in terms of size, color and quality. In addition, many of the provided signs are not easily visible from the road due to the parking and storage problems for the surplus of vehicles in the area.

Utilities

Electrical power is provided to the area through above-ground utility wires that are located on the north edge of Waverly Avenue from Fenimore Road to Concord Avenue, and along the south edge from Concord Avenue to Rockland Avenue. For the most part, these power lines are sited near the curb edge of the sidewalk, but in a few locations, the utility poles are effectively sited at the building edge of the street, as there is no curb edge. In these cases, where utility lines are located between buildings and parked cars without a defined curb edge, the pedestrian space is reduced even further, as drivers park their cars between the utility poles.

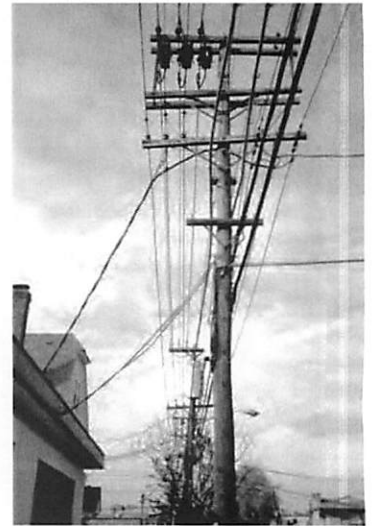
Parking

Parking is a major issue along Waverly Avenue and on the immediate side streets. This problem was identified in the Village Industrial Area Study which noted that the parking shortage leads to cars parking within non-designated spaces such as sidewalk areas and the road right-of-way. The report also noted, however, that the severity of the problem varies in different sections of the industrial parks, with some subareas being congested with cars while others have on-street parking space available.

Previous parking analyses carried out by the Village Police Department in 1996 and by the Village Traffic Department in 1997 (the Paonessa Report) examined parking provision in the area and also noted various violations related for example to illegal diagonal parking and overnight parking. The Paonessa study surveyed parking utilization on streets serving the light industrial park, showing that on-street parking during the business day was almost fully utilized, as noted in the table below:

Street	Available	Occupied
Waverly	124	124 spaces
Center	120	96 spaces
Fayette	128	118 spaces
Ogden	46	46 spaces
Concord	36	34 spaces

The demand for parking space has not lessened. A recent weekday parking analysis (June 2004) showed 160 cars parked along Waverly Avenue. Between 35 and 40 of these cars were parked perpendicular to the travel lanes, creating potentially hazardous conditions for drivers.



Urban Design Concerns

The unclear delineation of sidewalks and roadways is immediately noticeable along Waverly Avenue. As described, there are many places along Waverly Avenue where the distinction between the roadway, the sidewalk, and private property is blurred both by the lack of a curb edge and by the parked cars that overlap public and private properties.

Additionally, some of the auto-related businesses use the sidewalk/parking area for long term car storage. These uses often reduce the roadway width along Waverly Avenue, and take over precious parking areas. Cars that are parked perpendicularly to the street not only occupy the space that should be reserved for pedestrians, but can create dangerous situations when they reverse into the oncoming traffic lanes.

Adequate property maintenance is another problem that needs to be addressed in the Mamaroneck Industrial Area. Waverly Avenue could be dramatically improved by basic maintenance efforts on the part of property owners and through strict code enforcement carried out by the Village Building Department. Some property owners have made an effort to replace cracking asphalt with brick pavers and landscaping, to create a pleasant sidewalk.

Urban industrial areas are often in conflict with the neighboring residential property owners, and Mamaroneck is no exception. The views from the residential neighborhood are marred by the local industrial activity, and noxious odors produced by local businesses and the nearby Transfer Station occasionally drift into the residential areas. While little can be done about the odors so long as the businesses remain in the area, a streetscape program for Waverly Avenue will help with property maintenance and will make for a smoother visual and physical transition to the neighboring residential areas, an observation made in the Westchester County Department of Planning, study on the Mamaroneck Village Industrial Area Study, July 1997.



Cars park over the curb , even where there is a new sidewalk



Poorly maintained sidewalk



Brick pavers and landscaping installed and maintained by property owner



Poorly maintained sidewalk

3. Streetscape Recommendations

Goals

The goals for improving the Waverly Avenue streetscape include:

- Eliminating privately stored cars along the public right-of-way and on individual property “front yards” unless they are part of a planned or approved parking lot.
- Providing clearly marked parallel parking spaces on both sides of the Avenue.
- Improving pedestrian access through the creation of sidewalks and curbing.
- Limiting the number of driveways onto Waverly Avenue. Where possible, each business should have a maximum of one driveway that opens directly onto Waverly Avenue. Supplemental driveways can be provided off of side streets where access is available.
- Improving the appearance of the street through tree planting, new lighting and other landscape treatment, ensuring coordination with the streetscape proposals for Fenimore Road.

On-Street Parking

A first step towards distinguishing the public roadway from the private properties would be to create the curb edge along the roadway and to enforce parking regulations that prohibit parking on areas that should be designated as sidewalks. In addition, paved or hard surface areas that exist within the “front yard” areas of properties should be landscaped and maintained as permanent green areas. These changes would create more space for sidewalks, improved safety for pedestrians and drivers, and a clear delineation of public and private space.

The introduction of curbing and the designation of clearly marked parallel parking bays will be a key step in the future upgrading of the Waverly Avenue industrial park area. Public parking spaces however will remain as a major need, since this action will eliminate the ability for drivers to park perpendicular to the travel lanes [as noted earlier, up to 25% of the parking was found to be perpendicular to Waverly Avenue]. The resulting reduction in off-street parking spaces will mean that on-street facilities should be used exclusively to meet parking needs and not for the long-term storage of vehicles related to local auto sales and service operations. The Village can address this issue in two ways:

- (1) By prohibiting overnight parking (e.g. 3 am – 6 pm) on streets in the area.
- (2) By installing metered parking.

The first option would involve little initial outlay for the Village, and is likely to be less controversial to local businesses. The Village police department would be responsible for enforcement through patrols and random ticketing. Restrictions placed on overnight parking will not, however, deal with the problem of daytime storage of vehicles where scarce on-street space could be better utilized for business parking.

Metering

Metered parking could involve the use of standard curbside meters or "Pay and Display" metering. Standard metering is less difficult to enforce and easier to maintain than the 'Pay and Display' metering which is subject to breakdowns and more difficult to administer.

The use of meters on Waverly Avenue would significantly reduce or eliminate the curbside storage of vehicles for protracted periods of time. Metering would also provide income which should be set aside for the acquisition and construction of public parking areas which would serve the businesses in the area. Village action is required to ensure that the net income generated by the metering of parking on Waverly Avenue and Fenimore Road is set aside to implement this new parking space.

The Village should consider adopting long term (8-12 hour) meters along Waverly Avenue, while side streets would remain unmetered but would prohibit overnight parking. This strategy would allow employees/ owners to park on-street near businesses, but would discourage indiscriminate vehicle storage. In addition shorter-term parking (1-2 hours) could be considered on portions of Fenimore Road for use by customers and visitors to the commercial uses on Fenimore Road. This could include short-term meter parking along portions of the west side of Fenimore Road (where most of the retail and commercial uses are located) and along the first 200 feet to the west of the Waverly Avenue/Fenimore Road intersection. Long-term meter parking could occur for the balance of spaces on Fenimore Road and on Waverly Avenue.

Street Trees

Street trees should be planted along Waverly Avenue where possible. There are few street trees along Waverly Avenue today, and those that are there, are maintained by private property owners. Most of these trees are small, but a few, particularly at the north west corner of the area, are large enough to be seen from a distance. These trees make a tangible difference in upgrading the image of the area and the addition of new street trees will further improve the image of the Avenue.

The maintenance of street trees can often be a problem, particularly in areas with overhead utility wires and little other landscaping. As overhead utility wires are unlikely to be replaced by underground wiring in the near future, the design elements need to accommodate existing wiring. For example, using narrow crown style trees on the side of the street where there are utility wires will reduce the need to prune trees for utility maintenance. Examples could include Redspire Flowering Pear and Serviceberry. Similar species of trees should also be selected for Fenimore Road in order to maintain a coordinated design approach for this section of the light industrial park.

Signage

In order to create a sense of cohesion and identity for the businesses located within the Mamaroneck Industrial Area, the village should consider the use of a logo and repetitive sign design for the industrial area as a whole. This logo could be used on signs indicating off-site parking, location and directional maps, and gateways.

Curb Cuts

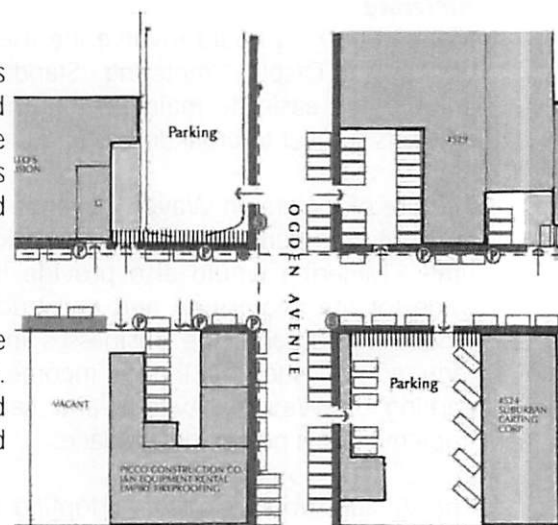
For safety, traffic flow and aesthetic reasons it is important to manage access along Waverly Avenue, to and from the adjacent properties. A large number of driveways, with indiscriminate left turns in and out of these driveways leads to an unsafe, unscenic and congested route. The following strategies should be considered:

1) Shared driveways: Whenever possible, adjacent property owners should be encouraged to share driveways. A positive example would be where several businesses share a single driveway, and have created interior driveways to aid in vehicle circulation between businesses. This allows the turning movements in and out of the driveway to be combined with the turning movements of the side street, thus improving traffic flow and safety along the street, and reducing the number of conflicts along the sidewalk.

2) Access from side streets: Whenever possible, access to private properties should be provided from a side street. Opportunities exist, for example, at Ogden Avenue, where access to parking areas could be developed off the side street rather than off Waverly Avenue.

Through Traffic Concerns

Much of the traffic along Waverly Avenue is due to through traffic that uses the street. Many cars and trucks use Waverly Avenue as a route to Fenimore Road, from which they access either of the main highways in Mamaroneck – the New England Thruway (I-95) or Boston Post Road. This through traffic causes congestion along Waverly Avenue, and often is as much the cause of traffic problems as the overflow loading and servicing issues from local businesses.



Opportunities exist for access off Ogden Avenue

Any improvements to the street should include the use of signage and traffic controls to discourage the use of Waverly Avenue as a through street. Particularly to the east of Fenimore Road, the through traffic for large trucks should be diverted on to Hoyt Avenue to Mamaroneck Avenue, which runs parallel to Waverly Avenue to the south, and to Boston Post Road via Fenimore Road for vans and smaller trucks (i.e. for vehicles unaffected by the low bridge height).

Signage should be installed at the Fenimore Road intersection directing traffic for Mamaroneck Avenue via Hoyt Avenue. In addition, a more prominent "No Trucks" sign, placed after the intersection and at the bridge over the Sheldrake River at Plaza Avenue, would help eliminate truck traffic from entering the residential part of Waverly Avenue.

Proposals

Two alternative streetscape schemes are shown graphically on the following pages. In the first of these schemes, the existing configuration of driveways is maintained, and new streetscape recommendations include the provision of one wide sidewalk along the north side of the Avenue. In the second scheme, some of the duplicate driveways have been omitted to help create a more pedestrian-friendly design, and sidewalks are provided on both sides of the street.

Both sets of proposals reflect the design specifications shown for the Fenimore Road Improvements prepared by Chas. H. Sells Inc. These include:

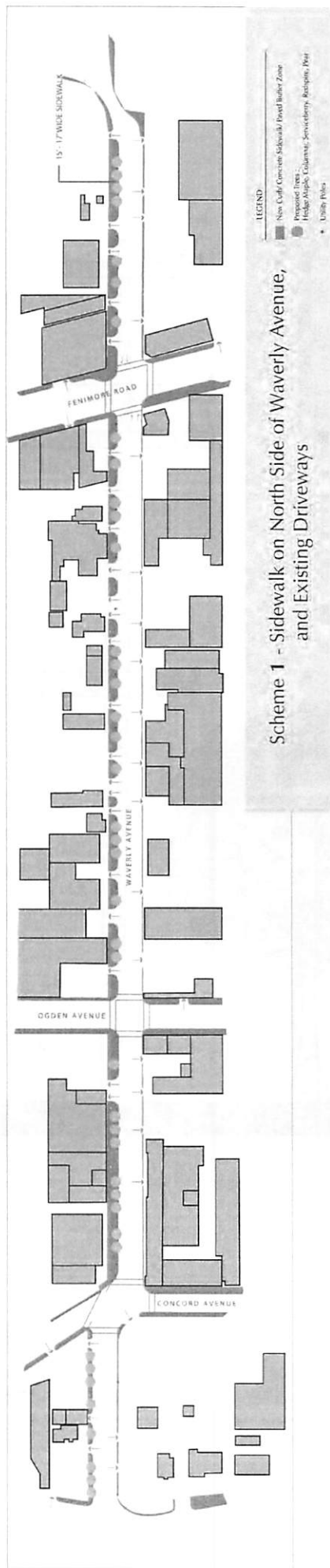
- Provision of new concrete sidewalks at a minimum width of 4'0" with a 2'0"-3'0" wide curbside strip to allow for light poles, signage and for snow dumping.
- 20'0" to 22'0" long bays for on-street parallel parking
- Planting of street trees which should be a columnar variety to reduce conflicts with overhead utility lines and to limit overhang beyond sidewalks.
- Additional landscaping in areas between the rear of the sidewalk and buildings (unless part of a planned and approved parking lot). These "front yard" areas would be planted and maintained by individual property owners. This may require a modification of the zoning code in order to make it mandatory and enforceable.
- New street lighting utilizing the model used in the downtown area and included in the Fenimore Road Improvements Plan.

Scheme 1

In the first alternative the multiple existing driveways are maintained, and a wide sidewalk (including curbside strip) averaging 16 feet wide, is located along the north side of the street. (Figure 2) By providing for a single large sidewalk, it is possible to locate the utility poles on the sidewalk rather than in parking areas and on the edge of travel lanes, as they are now. Due to the increased sidewalk width, there is ample room for tree planting, signage and light poles on the north side of the street. Figure 3 shows a cross section of the northern sidewalk design for Scheme 1. With this scheme, the street trees and lighting can be located along the curb edge, with utility poles set back by several feet.

On the southern side of the street there is only a curb edge, with a 2'0" strip to allow for snow dumping. Pedestrians would be encouraged to use the northern sidewalk through signage and street markings. Parallel on-street parking is provided on both sides of the street utilizing marked parking bays. 130 on-street parking spaces, each 20'0" to 22'0" long, are provided along Waverly Avenue.

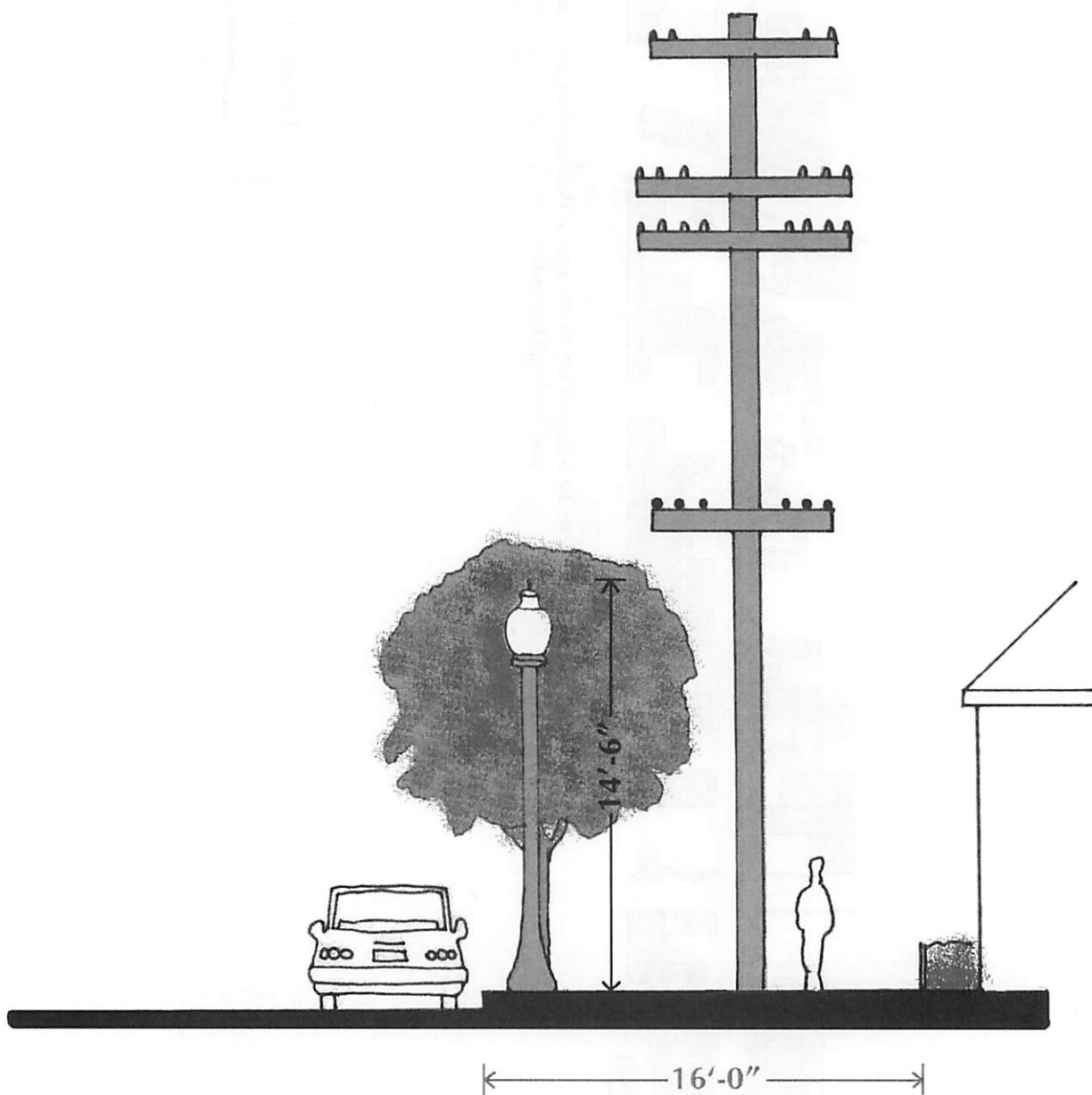
Figures 4a and 4b illustrate the impacts of the design improvements described by Scheme 1.



Study Area

WAVERLY AVENUE
Mamaroneck, NY

Figure 2



WAVERLY AVENUE
Mamaroneck, NY

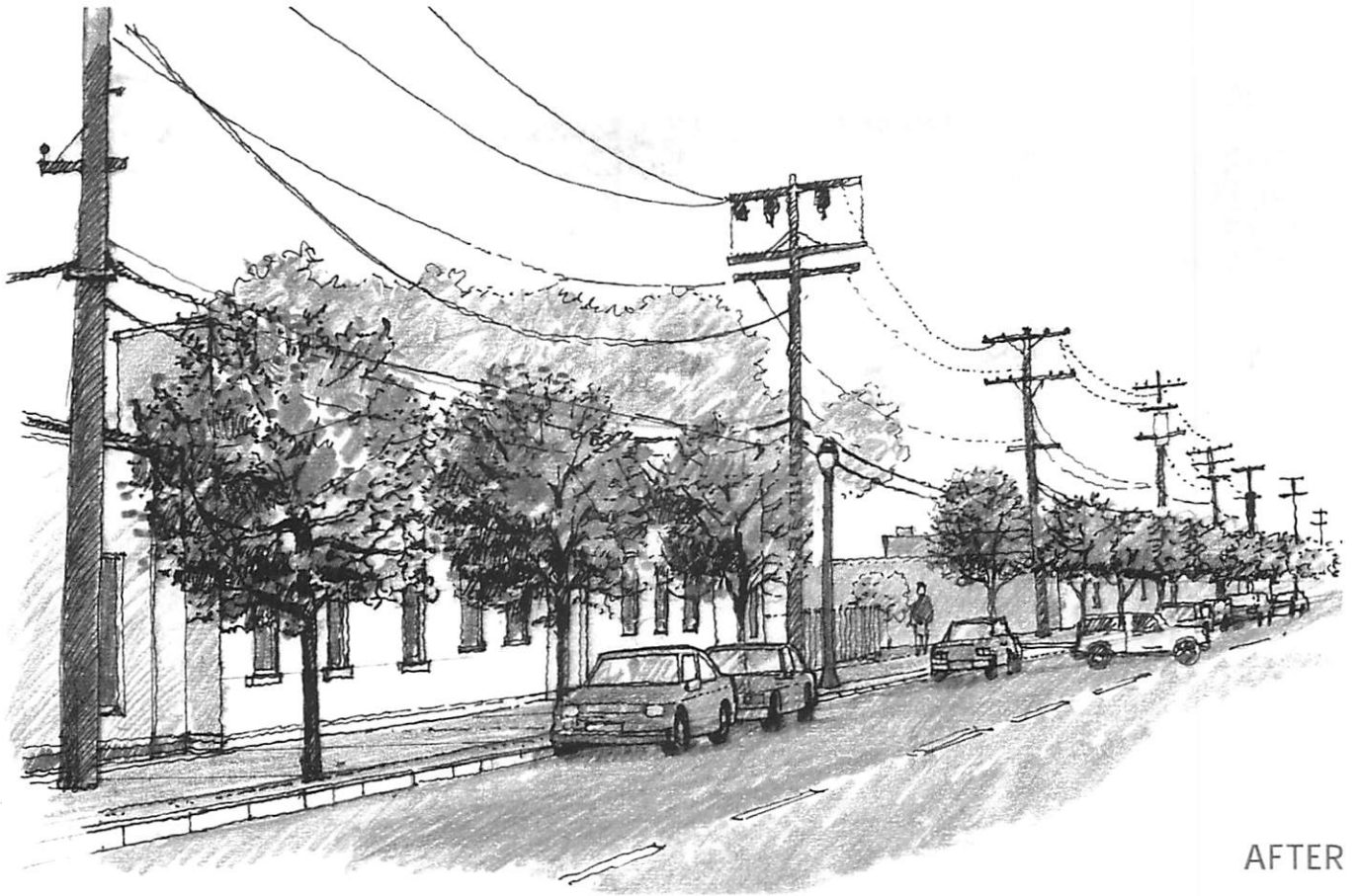
Figure 3

0 60 120 ft

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BEFORE



AFTER

Figure 4

WAVERLY AVENUE
Mamaroneck, NY

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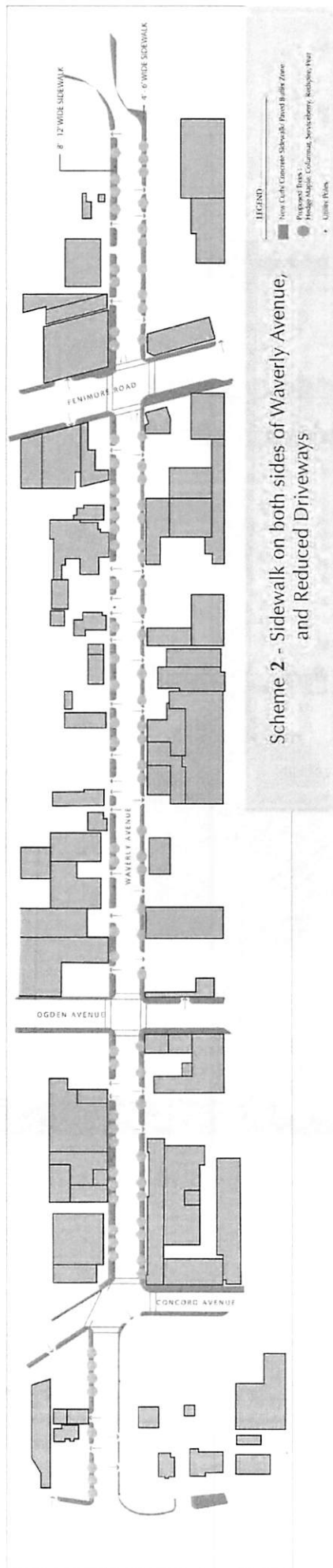
Scheme 2

The second scheme for Waverly Avenue illustrates an alternative with sidewalks on both sides of the street. (Figure 5) Sidewalks average approximately 6'0" on the south side of Waverly Avenue and 10'0" on the north side (including curbside strips). The number of driveways on Waverly Avenue is also reduced in Scheme 2 by requiring businesses to use driveways on side streets where possible, and by eliminating driveway duplication. This means that on-street parking can be increased, with 140 spaces provided. With the additional sidewalk width on the south side of the street, there is sufficient room for tree plantings on both sides of Waverly Avenue, although conflicts with utility poles and lines, as well as curb cuts, reduce the number of potential locations for trees.

Figure 6 shows a cross section of the northern sidewalk design for Scheme 2. With this design alternative, street lighting, street trees and utility poles all fall within the same strip between the curb edge and the sidewalk.

Comparison

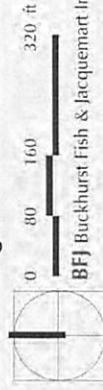
	<i>Scheme 1</i>	<i>Scheme 2</i>
<i>On-street Parking Spaces</i>	130	140
<i>Street Trees</i>	45	95
<i>Driveways</i>	Existing layout	Reduced
<i>Pedestrian Sidewalk</i>	North side	Both sides



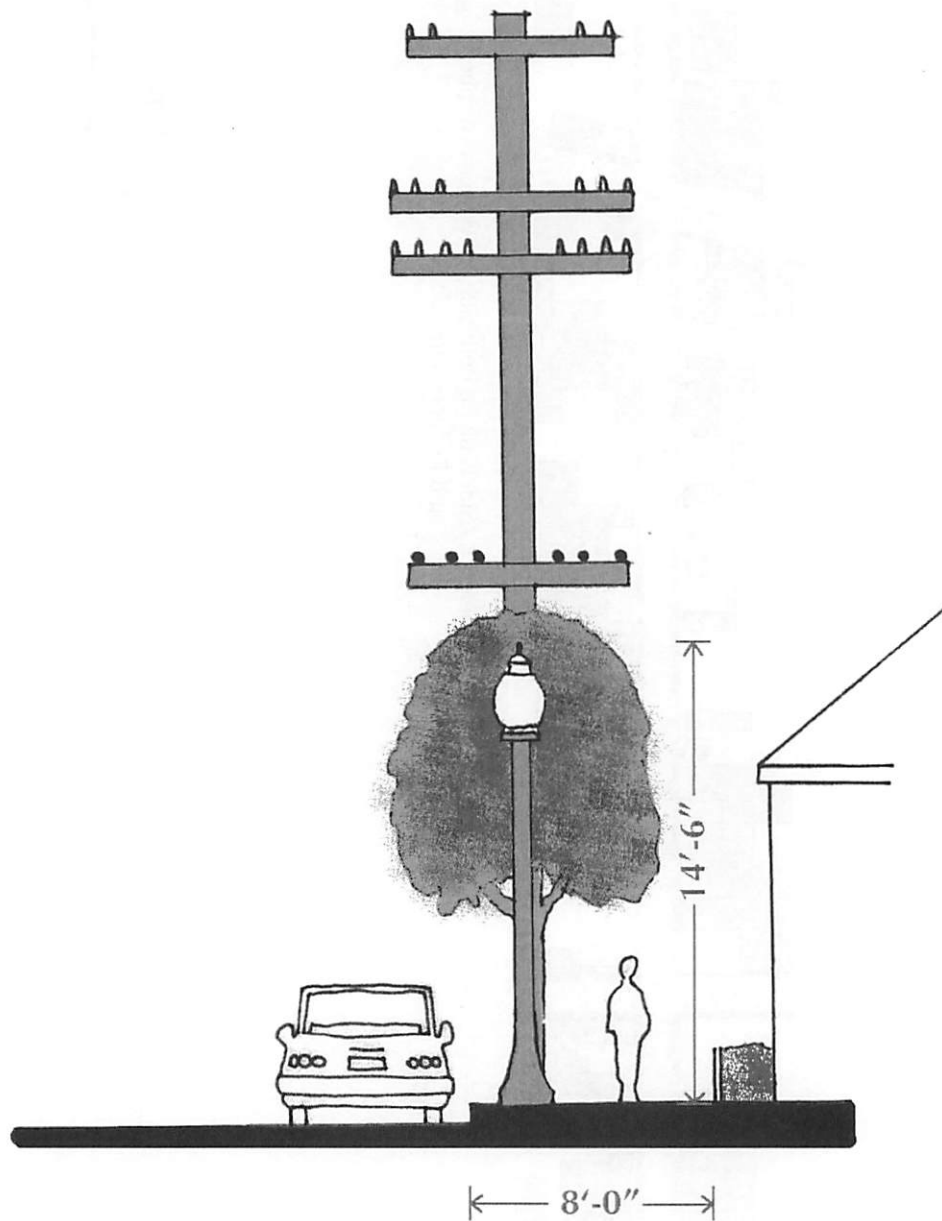
Study Area

WAVERLY AVENUE
Mamaroneck, NY

Figure 5



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WAVERLY AVENUE
Mamaroneck, NY

Figure 6

0 60 120 ft

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